

Town of Halifax, Vermont – Invitation to Bid 2026 Halifax Bridge No. 12 Branch Road Repair Project

Sealed Bids Due on or before June 2, 2026

Bid Process:

Sealed bids for provision as listed below will be accepted as follows:

- **If hand delivered:** Any time prior to 3:00pm on **June 2, 2026** in the Halifax Town office (246 Branch Road Halifax, VT) OR bidder may bring sealed bid to the 6:00pm Selectboard meeting.
- **If by mail:** Please send to Town of Halifax, PO Box 127 Halifax VT 05358 any time prior to 3:00pm on the bid due date.
- All sealed bids must be clearly marked:

Attention: Town of Halifax – Selectboard
Halifax Town – Halifax Bridge # 12 - 2026 Repair Project

The bids will be read publicly at the Selectboard meeting at **6:00pm on June 2, 2026**

Any contractor wishing to have a look at the project please feel free to call and set up a time to meet the Road Supervisor who will accompany you on a site visit. Please contact Mike Fournier at (802) 368-2803 to arrange a convenient time.

The Town of Halifax (“The Town” or “Halifax”) reserves the right to reject any and all bids, and to accept other than the apparent lowest bid price as read, to amend, modify, or withdraw this request for bids, to request submittal of supplemental documents or information from any and all bidders. The Town reserves the right to waive any minor or non-material formalities, if deemed to be in the Town’s best interest.

The Town will advertise in the town agreed newspaper (Deerfield Valley News), as well as the Town website.

halifaxvt.com.

- Please see **Attachment A (page 2)**, Project description and specifications, for providing your bid. Please note any exceptions or additions made to bid specifications.
- Please see **Attachment B (page 4)**, complete and return the Bid Form - Town of Halifax, Vermont.
- Please see **Attachment C (page 6)**, VTrans Bridge Inspection Report dated July 1, 2024 (18 pages).
- Please see **Attachment D (page 25)**, Halifax Purchasing Policy, for providing your bid. The Purchasing Policy is also available on the Town Website.

<https://halifaxvt.com/wp-content/uploads/2022/10/Purchasing-Policy.pdf>

Any questions please reach out to Michael Fournier by phone (802) 368-2803

Attachment A PROJECT DESCRIPTION

The Town of Halifax is requesting sealed bids for the 2026 Halifax Bridge No. 12 Branch Road Repair Project (the "Project"). The bridge is located at the intersection of VT Route 112 and Branch Road in Halifax, VT. The VTrans location is described as:

- Route: C2001 Structure # 00012
- C2001 over East Branch North River
- Last Inspection Date: July 1, 2024
- A copy of the bridge inspection report is included below

Contractor will provide all labor, equipment, and materials, including safety measures and traffic control. All of which must comply with current State of Vermont regulations. Please see "Other Requirements" below for certain specific requirements that are the contractor's responsibility.

SCOPE OF WORK

Construction Details:

As included in the VTrans Bridge Inspection Report:

- Downstream wingwall at abutment #2 has severe full height vertical crack that has continued to progress with measurements taken at the top area measuring ~2" in separation and ~7/8" of backwards translation. Inspection in 2022 had ~1-3/4" of separation and ~3/4" of backwards rotation while the 2020 inspection measurements were ~1-1/4" wide at the top and ~3/8" of backwards translation. Remaining wings have some minor cracking with some light efflorescence leakage and concrete scaling along the front faces.
- Retention measures should be taken to restrain the downstream wingwall (wingwall #4) of abutment #2 as wide full height vertical crack is present with up to ~2" of separation and ~7/8" of backwards settlement take near the top or wingwall should be fully replaced.
- At this time, the Town of Halifax seeks bids to implement retention measures rather than full wingwall replacement.

Other Requirements:

- The contractor will provide equipment, operators and laborers necessary complete the project in accordance with AoT standards.
- The contractor shall be responsible for all cleaning of the road surface prior after work as necessary.
- The contractor is responsible for locating on or off project sites for this activity. All waste material generated from this activity shall be removed from the project within 48 hours upon completion of the project at the sole expense of the contractor.
- The contractor is expected to work within the Town right of way. Any cost associated with the contractors desire to obtain clearances to work outside the existing right of way will be at the sole expense of the contractor.
- Damage by the contractor to Town and or personal property, or utilities during construction will be repaired by the contractor to pre-construction condition and will be at the sole expense of the contractor.
- The contractor is required to provide traffic control for the duration of the project. A traffic control plan will be required, all traffic control shall be compliant with the Manual on Uniform Traffic Control Devices (MUTCD) Standards.

BIDS

As noted on the first page of the Invitation to Bid:

Bidders will provide all labor, equipment, and materials, including safety measures and traffic control. All of which must comply with current State of Vermont regulations. Please see "Other Requirements" below for certain specific requirements that are the contractor's responsibility.

1. Sealed bids for provision as listed below will be accepted as follows:
 - **If hand delivered:** Any time prior to 3:00pm on **June 2, 2026** in the Halifax Town office (246 Branch Road Halifax, VT) OR bidder may bring sealed bid to the 6:00pm Selectboard meeting.
 - **If by mail:** Please send to Town of Halifax, PO Box 127 Halifax VT 05358 any time prior to 3:00pm on the bid due date.
 - All sealed bids must be clearly marked:
Attention: Town of Halifax – Selectboard
Halifax Town – Halifax Bridge #12 - 2026 Repair Project

The bids will be read publicly at the Selectboard meeting at 6:00pm on **June 2, 2026**

2. The Project shall be completed by **October 15, 2026**
3. Upon completion, the Project shall comply with all Agency of Transportation standards and guidelines.

PRE BID SITE VISIT

Any contractor that wishes to have a look at the Project please feel free to do so. In the event you would like to discuss the Project please call and arrange a time to meet with the Town's Road Supervisor who will accompany you or in the event he is unable to join you will arrange for one of the Road Commissioner's to join you on a site visit. Please contact Mike Fournier at (802) 368-2803 to arrange a convenient time.

INSURANCE

The Town of Halifax will require the contractor to have insurance and maintain in effect Worker's Compensation insurance within statutory limits and General Liability Insurance to cover bodily injury and property damage, adequate to protect the Town against liability for bodily injury or death of any person in connection with the Project in an amount not less than the amounts shown below.

GENERAL LIABILTY	\$1,000,000.00
AUTOMOBILE LIABILTY	\$1,000,000.00
UMBRELLA LIAB	\$2,000,000.00
WORKERS COMPENSATION & EMPLOYERS LIABILTY	\$500,000.00

Further, the contractor will name the Town of Halifax as the named insured in amounts not less than the above amounts and will provide a copy of the insurance certificate to the Town before any work is performed.

Attachment B

**Bid form - Town of Halifax, Vermont
Sealed Bids Due on June 2, 2026**

Date Bid Submitted: _____
Company name: _____
Address: _____

Authorized Contact Name: _____
Authorized Contact Title: _____
Contact Telephone Number: _____
Contact Email Address: _____

Attention:
Town of Halifax Selectboard
PO Box 127
Halifax, VT 05358

Dear Halifax Selectboard,

Having examined the documents provided with the subject invitation, and conducted a site visit if necessary the undersigned proposes to provide the Town of Halifax the following in accordance with the subject documents.

As an Authorized Signatory for (Please Insert Company Name): _____ I submit the following 2 page bid to complete the described work for the Green River Road 2025 Paving Project for Town of Halifax, VT:

1. Bid Amount for Contracting Services:

Total Bid amount in \$ _____ Total Bid Amount in Words : _____

2. Please provide a detailed proposal of your scope of work with your letterhead. Please include a description of all work to be done and materials used in the repair of Bridge No. 12.

3. Please indicate your/your Company's ability to provide the Town of Halifax with:

- An Insurance Certificate prior to commencement of the Project. **(Circle) Yes / No**

4. Estimated Date to Commence Work (MM/DD/YYYY) _____

5. Target Date of Completion (MM/DD/YYYY): _____

6. Please attach any additional commentary or documentation relevant to your bid.

The contractor agrees to furnish the necessary labor, equipment and material to perform all work in accordance with the request for bid, additional specifications, project documents, the State of Vermont Agency of Transportation standard specifications for construction 2018, and its latest revisions, and the latest version of the MUTCD.

The contractor shall warranty materials and workmanship for a period of one year from the date of completion. Defects and associated repair shall be reviewed with the Town prior to the remedial work. All remedial work shall be completed by the contractor at no additional cost to the Town.

I understand the Town of Halifax, VT reserves the right to reject any and all bids, and to accept other than the apparent lowest bid price as read, to amend, modify, or withdraw this request for bids, to cancel any and or all projects prior to the start of construction without prior notification, to request submittal of supplemental documents or information from any and all bidders. The Town reserves the right to waive any minor or non-material formalities, if deemed to be in the Town's best interest.

I certify, under penalties of perjury, that (1) I have had the opportunity to view the full bid package and am aware it was my responsibility to perform my own due diligence appropriate to submitting this proposal, (2) I am fully authorized to submit this bid, (3) I have not engaged in discussions, negotiations, or collusion with any person to determine what my bid will be and (4) that I, to the best of my knowledge and belief, have paid all taxes, fees, assessments, betterment or other municipal charges that I owe to the Town of Halifax or have payment agreement in place or have filled an appeal over the same.

Authorized Signature of Bidder:

Title of Authorized Signatory:

Date Signed (MM/DD/YYYY):

Attachment C

VTrans Bridge # 12 Inspection Report dated July 1, 2024

The entire report follows on pages 7 -24

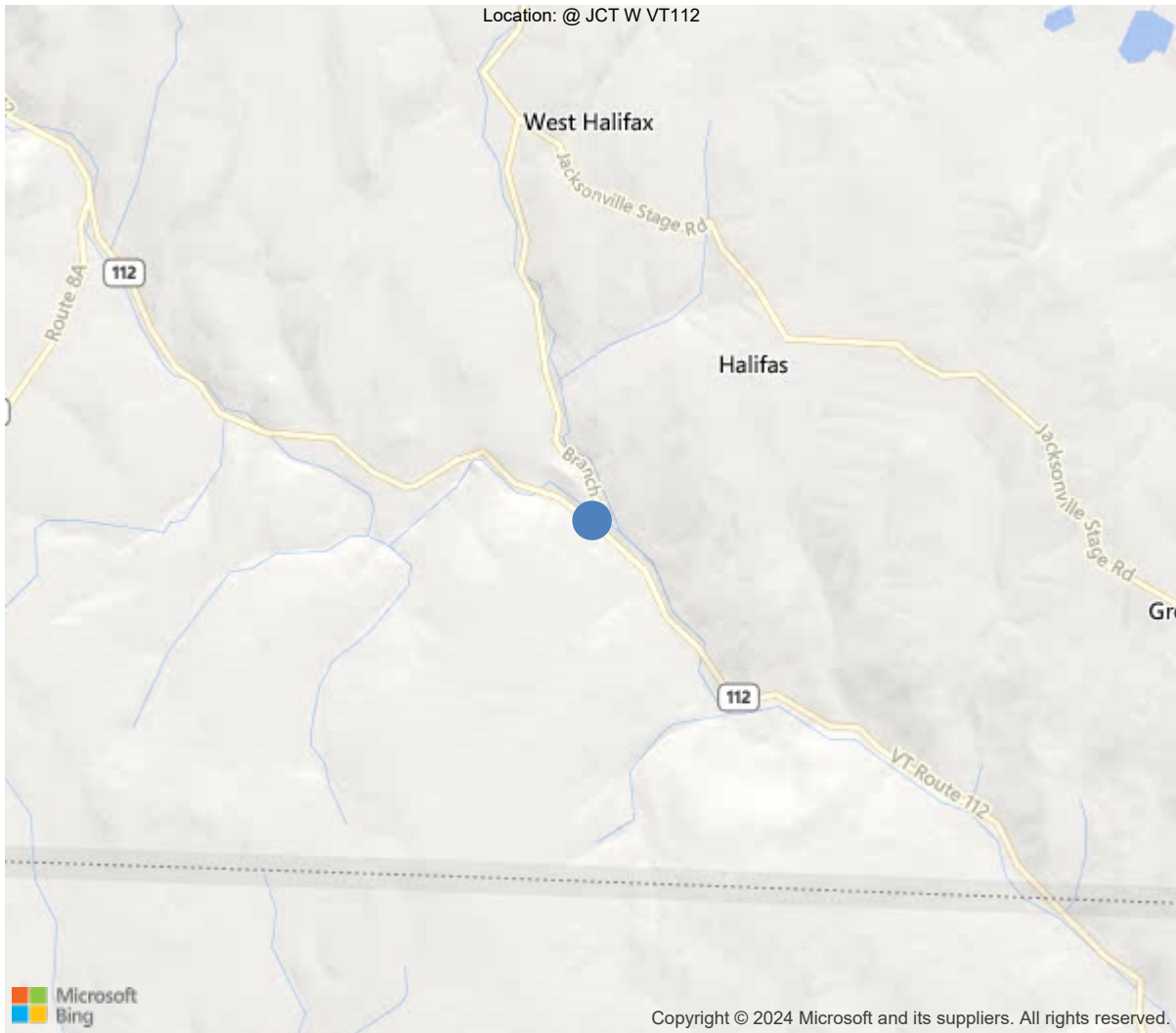


Town: 93 - HALIFAX

District 2, 25 - WINDHAM County

Owner: 3 - Town or Township Highway Agency

Maintenance Responsibility: 3 - Town or Township Highway Agency



42.75841, -72.76140

IDENTIFICATION	
(1) State Names	50 - Vermont
(8) Structure Number	101308001213081
(5) Inventory Route	1
(2) Highway Agency District	2 - District 2
(3) County Code	25 - WINDHAM
(4) Place Code	31150
(6) Features Intersected	EAST BR NORTH RIVER
(7) Facility Carried	C2001
(9) Location	@ JCT W VT112
(11) Mile Point	0 mi
(12) Base Highway Network	No
(13) LRS Inventory Rte & Subrte	
(16) Latitude	42.7584138888889
(17) Longitude	-72.7614
(98) Border Bridge State Code	
(99) Border Bridge Structure No.	
STRUCTURE TYPE AND MATERIAL	
(43) Main Structure Type	14
Material	1 - Concrete
Type	4 - Tee beam
(44) Approach Structure Type	00
Material	0 - Other
Type	0 - Other
(45) No. of Spans in Main Unit	1
(46) No. of Approach Spans	0
(107) Deck Structure Type	1 - Concrete Cast-in-Place
(108) Wearing Surface/Protective System	
Type of Wearing Surface	6 - Bituminous
Type of Membrane	0 - None
Type of Deck Protection	0 - None
AGE AND SERVICE	
(27) Year Built	1925
(106) Year Reconstructed	1981
(42) Type of Service	15
On	1 - Highway
Under	5 - Waterway
(28) Lane	
On	2
Under	0
(29) Average Daily Traffic	190
(30) Year of ADT	2019
(109) Truck ADT	3 %
(19) Bypass, Detour Length	3 mi
GEOMETRIC DATA	
(48) Length of Maximum Span	38 ft
(49) Structure Length	42 ft
(50) Curb or Sidewalk Width	
Left	0.3 ft
Right	0.3 ft
(51) Bridge Roadway Width Curb to Curb	26.8 ft
(52) Deck Width Out to Out	28.3 ft
(32) Approach Roadway Width (W/Shoulders)	21 ft
(33) Bridge Median	0 - No median
(34) Skew	0 Deg
(35) Structure Flared	0 - No flare
(10) Inventory Route Min Vert Clear	99.99 ft
(47) Inventory Route Total Horiz Clear	26.8 ft
(53) Min Vert Clear Over Bridge Rdwy	99.99 ft
(54) Min Vert Underclear	0 ft
Ref:	
(55) Min Lat Underclear RT	0 ft
Ref:	
(56) Min Lat Underclear LT	0 ft
NAVIGATION DATA	
(38) Navigation Control	0 - No navigation control on w
(111) Pier Protection	
(39) Navigation Vertical Clearance	0 ft
(116) Vert-Lift Bridge Nav Min Vert Clear	0 ft
(40) Navigation Horizontal Clearance	0 ft

CLASSIFICATION	
(112) NBIS Bridge Length	Y
(104) Highway System	0
(26) Functional Class	8 - Rural Minor Collector
(100) Defense Highway	0 - The inventory route is not
(101) Parallel Structure	N - No parallel structure exists
(102) Direction of Traffic	2 - way traffic
(103) Temporary Structure	
(105) Federal Lands Highways	0 - N/A
(110) Designated National Network	0 - The inventory route is not
(20) Toll	3 - On free road. The structure
(21) Maintain	3 - Town or Township Highway A
(22) Owner	3 - Town or Township Highway A
(37) Historical Significance	5 - Bridge is not eligible for
CONDITION	
(58) Deck	6
(59) Superstructure	6
(60) Substructure	6
(61) Channel & Channel Protection	6
(62) Culverts	N
LOAD RATING AND POSTING	
(31) Design Load	0 - Other or Unknown
(63) Operating Rating Method	1
(64) Operating Rating	
Type	1 - Load Factor(LF)
Rating	26
(65) Inventory Rating Method	1 - Load Factor(LF)
(66) Inventory Rating	
Type	
Rating	15
(70) Bridge Posting	5 - Equal to or above legal loads
(41) Structure Open/Posted/Closed	P - Posted for load (may include
APPRAISAL	
(67) Structural Evaluation	4
(68) Deck Geometry	5
(69) Clearances, Vertical/Horizontal	N
(71) Waterway Adequacy	6
(72) Approach Roadway Alignment	8
(36A) Bridge Railings	1 - Inspected feature meets current
(36B) Transitions	1 - Inspected feature meets current
(36C) Approach Guardrail	1 - Inspected feature meets current
(36D) Approach Guardrail Ends	0 - Inspected feature does not meet
(113) Scour Critical Bridges	8 - Bridge foundations determined to
PROPOSED IMPROVEMENTS	
(75) Type of Work	35 - Bridge rehabilitation bec
(76) Length of Structure Improvement	42 ft
(94) Bridge Improvement Cost (Multiply value by 1000)	\$ 416
(95) Roadway Improvement Cost (Multiply value by 1000)	\$ 50
(96) Total Project Cost (Multiply value by 1000)	\$ 466
(97) Year of Improvement Cost Estimate	2020
(114) Future ADT	200
(115) Year of Future ADT	2029

INSPECTIONS *			
(90) Inspection Date			07/01/2024
(91) Frequency			24
(92) Critical Feature Inspection	Done	Freq. (Mon)	Date
A: Fracture Critical Detail	No		
B: Underwater Inspection	No		
C: Other Special Inspection			
* The inspection date and frequency information in this box contains the current NBI date and frequency information. Please refer to the report header for the date this inspection was conducted.			

Maintenance Needs

Date Reported: 07/07/2022

Priority: 4 - Maintenance Finding - Next Inspection Cycle

Status: Open

Type of Work: 34 - Substructure - Wing/Retaining wall repair or (re)construction

Component: Substructure

Deficiency Description

Downstream wingwall at abutment #2 has severe full height vertical crack that has continued to move with measurements taken at the top area measuring ~2" in separation and ~7/8" of backwards translation. Retention measures should be taken to restrain the downstream wingwall of abutment #2 as wide full height vertical crack is present or wingwall should be fully replaced.

Remarks



Wingwall #4



Wingwall #4 Settlement Crack

Deck

ELEMENTS	DESCRIPTION	UNITS	TOTAL	CS1	CS2	CS3	CS4
12	Reinforced Concrete Deck	SF	336	336	0	0	0
510	Wearing Surfaces	SF	273	189	84	0	0
3220	Crack (Wearing Surface)	SF	84	0	84	0	0
16	Reinforced Concrete Top Flange	SF	853	807	25	21	0
1080	Delamination/Spall/Patched Area	SF	6	0	0	6	0
1120	Efflorescence/Rust Staining	SF	40	0	25	15	0
510	Wearing Surfaces	SF	853	853	0	0	0
330	Metal Bridge Railing	LF	84	72	12	0	0
1000	Corrosion	LF	6	0	6	0	0
7000	Damage	LF	6	0	6	0	0
804	Concrete Fascia	LF	84	84	0	0	0

58 - Deck (6 - SATISFACTORY CONDITION - structural elements show some minor deterioration.)

Reinforced concrete t-beam deck that has been widened is in satisfactory condition. Small areas of wearing with some very light scaling and honeycombing are present along the interior bays with some being previously patched and areas of efflorescence leakage present. Small area where rebar is exposed due to lack of cover.

200 - Existing Wearing Surface Depth (8)

A21 - Deck Wearing Surface Condition (Satisfactory)

Asphalt is in satisfactory condition having some light wear and some minor longitudinal and transverse cracking along the downstream side that have been sealed. Abutment #2 side curb reveal measures 8" on downstream side and 12" on upstream side. Abutment #1 measures 1" down on downstream side and flush at upstream side. ~8" of asphalt present across deck.

A24 - Deck Curb Condition (Good)

Concrete curbing is in fairly good condition having some minor scrapes and gouges along the roadway face. Curb measures 12" high.

A39 - Deck Fascia Condition (Good)

Concrete fascia is in fairly good condition having some light wear.

B.C.05 Bridge Railing Condition Rating (GOOD - Some minor defects.)

Galvanized steel beam rail is in fairly good condition with a few light scrapes along the face of rail. Fascia mounted galvanized steel posts with steel offsets are in fairly good condition. Light bends from impact damage are present in posts #1 away from abutment #1 on the upstream side.

B.C.08 Bridge Joints Condition Rating (NOT APPLICABLE - Bridge does not have deck joints.)

APPROACH

72 - Approach Roadway Alignment (8 - Equal to present desirable criteria)

Roadway alignment is fairly straight with T-intersection with a slight elevation gain from abutment #2 to abutment #1.

A13 - Approach Rail Condition (Fair)

Team Lead: Stephen Piro, **Inspection Date:** 07/01/2024

Galvanized steel beam is in okay to fair condition. Heavy impact damage is present along the upstream side of abutment #1 side with heavy twists and bends in rail. Minor impact damage is present along the downstream side of abutment #1 with rail being flattened out with scrapes, dents and rusting.

A16 - Approach Post Condition (Fair)

Galvanized steel posts with composite offsets are present along the abutment #1 side only with heavy impact damage along the upstream side and minor impact damage along the downstream side. Upstream side has bent over posts and heavy twisting. Abutment #2 side has heavy duty creosoted timber posts with timber offsets in satisfactory condition having weathering with some cracks and splits.

B.C.06 Bridge Railing Transitions Condition Rating (FAIR - Some moderate defects; strength and performance of the component are not affected.)

Galvanized steel beam rail is in fair condition. Rail both upstream and downstream at abutment #1 end is heavily flattened out due to plow rub and impacts. Galvanized steel posts are present along the abutment #1 side with a mixture of timber and composite offsets while along the abutment #2 has creosoted timber posts and offsets having typical weathering present.

Superstructure

ELEMENTS	DESCRIPTION	UNITS	TOTAL	CS1	CS2	CS3	CS4
107	Steel Open Girder/Beam	LF	80	25	40	15	0
1000	Corrosion	LF	55	0	40	15	0
515	Steel Protective Coating	SF	500	500	0	0	0
110	Reinforced Concrete Open Girder/Beam	LF	120	104	0	16	0
1080	Delamination/Spall/Patched Area	LF	16	0	0	16	0
311	Movable Bearing	EA	2	2	0	0	0
313	Fixed Bearing	EA	2	2	0	0	0

59 - Superstructure (6 - SATISFACTORY CONDITION - structural elements show some minor deterioration.)

Three (3) reinforced concrete t-beams are in satisfactory condition with beam #1 have delaminations present mainly near the abutment #1 side along the lower portions of beam. Beams #2 and #3 have minor wear with some light scaling present and a few small rust stains. Structure was widened with reused steel beams that are in satisfactory condition having areas of minor previous pitting present with up to ~1/8" dimples present. Paint has some light paint distress with paint flaking and chipping around exterior steel beams.

A55 - Lateral Bracing Condition (Satisfactory)

Two (2) painted steel c-channel diaphragms are present in exterior bays only from deck widening project. Cross bracing is welded to L-angles that are welded to the webs of rolled beams and bolted into concrete t-beams. C-Channels have some light previous pitting present.

B.C.07 Bridge Bearings Condition Rating (SATISFACTORY - Widespread minor or isolated moderate defects.)

Steel bearings are in okay condition having rusting present.

Substructure

ELEMENTS	DESCRIPTION	UNITS	TOTAL	CS1	CS2	CS3	CS4
215	Reinforced Concrete Abutment	LF	57	43	14	0	0
1080	Delamination/Spall/Patched Area	LF	14	0	14	0	0
800	Reinforced Concrete Wing/Retaining Wall	EA	4	1	2	0	1
1080	Delamination/Spall/Patched Area	EA	2	0	2	0	0
1130	Cracking (RC and Other)	EA	1	0	0	0	1

60 - Substructure (6 - SATISFACTORY CONDITION - structural elements show some minor deterioration.)

Reinforced concrete abutment #1 is poured on ledge and is in satisfactory condition. Small voids are present in various areas between ledge and concrete. Areas of light honeycombing at cold joint across abutment stem. Upper portions below steel rolled beams #1 and #5 have previous concrete patches below and are holding up at this time. Upstream outer portions of stem and wingwall face (where deck was widened) have minor concrete scaling present.

Reinforced concrete abutment #2 is in satisfactory poured on concrete footing. Abutment stem has areas of light scaling and a few small cracks. Severe full height vertical crack is present at the downstream end of abutment along wingwall measuring ~2" at the top of the crack with wingwall having ~7/8" of backward translation away from abutment. Abutment continues to move with cracking between the downstream wingwall progressing. Previous concrete patch is present below steel rolled beam #1 is in sound condition while the downstream patched area below beam #5 has the full height vertical crack present.

A71 - Abutment End Walls Condition (Good)

Reinforced concrete t-beam backwall is in fairly good condition having minor wear.

A77 - Retaining/Wingwall Condition (Poor)

Downstream wingwall at abutment #2 has severe full height vertical crack that has continued to progress with measurements taken at the top area measuring ~2" in separation and ~7/8" of backwards translation. Inspection in 2022 had ~1-3/4" of separation and ~3/4" of backwards rotation while the 2020 inspection measurements were ~1-1/4" wide at the top and ~3/8" of backwards translation. Remaining wings have some minor cracking with some light efflorescence leakage and concrete scaling along the front faces.

A78 - Abutment Footings Condition (Fair)

Concrete footing is present along abutment #2 side only having heavy concrete scaling along the upper corner edge and minor abrasion along the channel face.

CHANNEL

61 - Channel Condition (6 - Bank is beginning to slump. River control devices and embankment protection have widespread minor damage. There is minor stream bed movement evident. Debris is restricting the channel slightly.)
 East Branch of North River is in fairly good condition flowing straight through structure flowing over boulders and ledge. Channel flows against the abutment #2 footing. Minor erosion is present along channel banks on the upstream side of abutment #2 with some eroded banks having fresh sediment exposed. Abutment #1 is poured on ledge. Abutment #2 has no protection. Channel banks have had recent stone riprap installed along the downstream side of abutment #2 and upstream of abutment #1 with bank stabilization measures. Light vegetation growth present along banks.

B.C.10 Channel Protection Condition Rating (SATISFACTORY - Widespread minor or isolated moderate defects.)

Abutments are poured on ledge with ledge exposed abutment #1 while abutment #2 ledge is below grade. Channel embankments both upstream and downstream past wingwalls have stone riprap and boulder mix with some vegetation growth forming.

B.C.11 Scour Condition Rating (Widespread minor or isolated moderate scour.)

Moderate localized scour is present in front of abutment #2 footing due to channel in direct contact with remnants of concrete footing.

GENERAL OBSERVATION

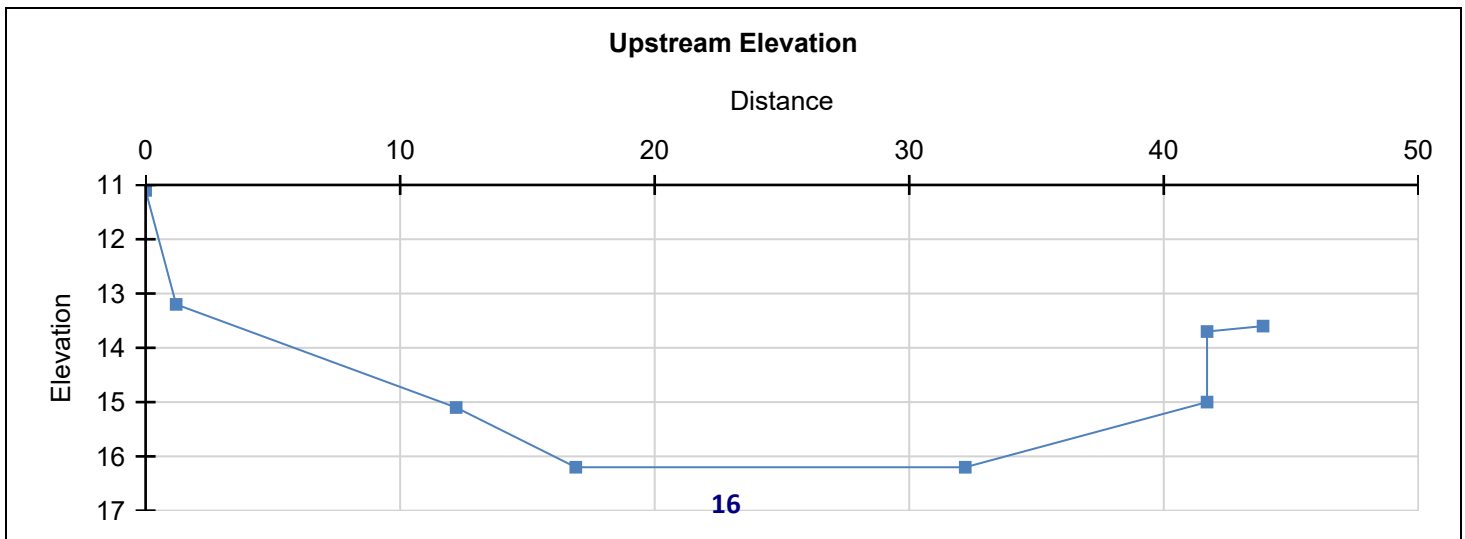
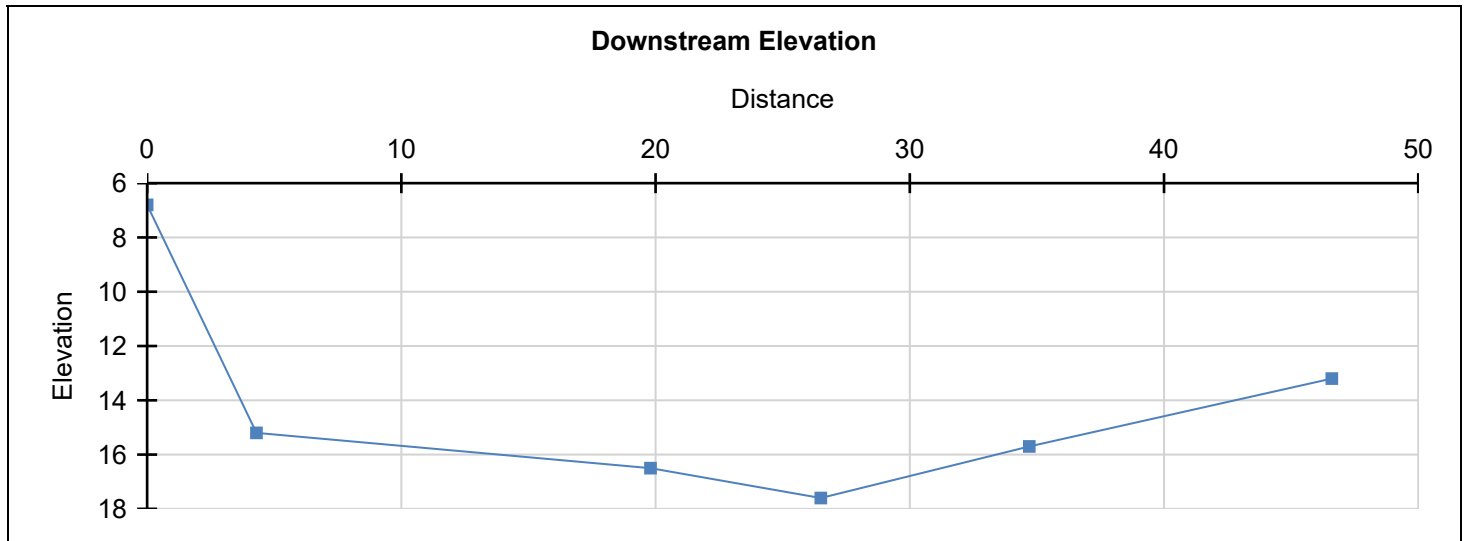
Damaged approach rail needs to be replaced past abutment #1 on both the upstream and downstream ends. Approach rail past abutment #2 on the downstream side is missing and should be installed with both the upstream and downstream sides being upgraded. Retention measures should be taken to restrain the downstream wingwall (wingwall #4) of abutment #2 as wide full height vertical crack is present with up to ~2" of separation and ~7/8" of backwards settlement take near the top or wingwall should be fully replaced.

Team Lead: Stephen Piro, Inspection Date: 07/01/2024

Channel Profile

Waterway Flow: Left to Right (With Stationing)	Top of Water:
Origin: Abutment #1 (Deck Underside)	Bottom of Beam:

Station	Distance	Downstream	Upstream
Abutment #1	0	6.8	11.1
Ledge	1.2		13.2
Bottom of Ledge	4.3	15.2	
Ledge	12.2		15.1
Channel Edge	16.9		16.2
Edge of Channel	19.8	16.5	
Deepest Point	26.5	17.6	
Channel Low	32.2		16.2
Edge of Channel	34.7	15.7	
Bottom of Footing	41.7		15
Top of Footing	41.7		13.7
Abutment #2	43.9		13.6
Abutment #2	46.6	13.2	





Footing along Abutment #2



Concrete T-Beams from Abutment #2



Abutment #1



Footing along Abutment #2



Abutment #2



Wingwall #4 Settlement Crack



Upstream Transition Rail



Downstream Transition Rail



Downstream elevation



Wingwall 4 at abutment 2



Wingwall 4



Wingwall 2



Soffit & superstructure bays 3 & 4 facing abutment
1



Downstream



Upstream



Soffit & superstructure bays 1 & 2 facing abutment
2



Abutment 1



Wingwall 3 & abutment 2 footing



Wingwall 3



Wingwall 1 & upstream retaining wall



Upstream elevation



Upstream fascia



Downstream fascia



Abutment 1 approach



Downstream approach & bridge rail at abutment 1 wingwall 2



upstream approach & bridge rail at abutment 1 wingwall 1



Downstream



Upstream



Wearing surface



Downstream approach & bridge rail



Approach rail to abutment 2 wingwall 3



Abutment 2 approach (East)

Attachment D

<https://halifaxvt.com/wp-content/uploads/2022/10/Purchasing-Policy.pdf>

PURCHASING POLICY Town of Halifax, Vermont

#1. PURPOSE

The purpose of this Purchasing Policy is to obtain the highest quality goods and services for the Town of Halifax at the lowest possible price, to exercise financial control over the purchasing process, to clearly define authority for the purchasing function, to allow fair and equal opportunity among qualified suppliers, and to provide for increased public confidence in the procedures followed in public purchasing.

#2. PURCHASING AUTHORIZATION

No purchase over \$24,999.99 or the limit of their budget, shall be made by any Town officer or employee without prior approval of the Selectboard. When making any purchase not subject to the bid process described below, officers and employees must solicit quotes from at least three vendors unless the Selectboard has approved a sole source vendor. Vendors will be selected based on cost, the quality of the goods and services offered, and the ability, capacity, and skill of the vendor demonstrated under prior contracts with the Town.

#3. BID PROCESS

A. All purchases of \$25,000.00 or more shall be subject to a formal bid process- The bid process shall be initiated by the issuance of a request for bids prepared by the Selectboard. Notice of the request for bids shall be made by letters to known providers soliciting bid responses, advertisements posted in three public locations within the Town, and advertisements placed in a newspaper of record for the town of Halifax.

B. There shall be a single contact person authorized for bidding procedures. Any additional bid information shall be issued to all bidders.

Any information disseminated by an unauthorized person shall be invalid.

C. Commodities.

1. Purchases of commodities shall include sand, gravel, fuel and heating oil. These purchases will be made from an approved vendor list by verbal or written communication. A minimum of three vendors will be contacted for bids. Newspaper ads are not required for commodity purchases.
2. Salt, tires, culverts, and plow cutting edges are purchased through a State bidding process.

#4. BID SPECIFICATIONS

Bid specifications shall include:

- Bid name
- Bid submission deadline
- Date, location, and time of bid opening
- Specifications for the project or services including quantity, design, and performance features Bond and/or insurance requirements
- Any special requirements unique to the purchase
- Delivery or completion date

Once a request for bids has been issued, the bid specifications will be available for inspection at the Town Office.

#5. BID SUBMISSION

All bids must be submitted in sealed envelopes, addressed to the Town in care of the Selectboard, and plainly marked with the name of the bid and the time of the bid opening. Bid proposals will be date stamped on the outside of the envelope immediately upon receipt. Any bid may be withdrawn in writing prior to the scheduled time for the opening of bids. Any bids received after the time and date specified shall not be considered and shall be returned to the bidder unopened.

Emailed or faxed bid submissions will not be accepted.

Bidders shall bid to specifications and any exceptions must be noted. A bidder submitting a bid thereby certifies that the bid is made in good faith without fraud, collusion, or connection of any kind with any other bidder for the same work, and that the bidder is competing solely on his/her behalf without connection with or obligations to any undisclosed person or firm.

#6. BID OPENING

Every bid received prior to the submission deadline will be publicly opened and read aloud by the Selectboard. The bid opening will include the name and address of bidder; for lump sum contracts, the lump sum base bid and the bid for each alternate; for unit price contracts, the unit price for each item and the total if stated; and the nature and the amount of security furnished with the bid if required.

#7. CRITERIA FOR BID SELECTION

In evaluating bids, the Selectboard will consider the following criteria:

- Price
- Bidder's ability to perform within the specified time limits.
- Bidder's experience and reputation, including past performance for the Town.
- Quality of the materials and services specified in the bid. Bidder's ability to meet other terms and conditions, including insurance and bond requirements.
- Bidder's submission of proof of worker's compensation insurance coverage for self and all employees.
Bidder's inclusion in bid packet of required certifications for all contractors, subcontractors, and suppliers.
Bidder's financial responsibility.

- Bidder's availability to provide future service, maintenance, and support.
Nature and size of bidder.
- Any other factors that the Selectboard determines are relevant and appropriate in connection with a given project or service.

The Selectboard reserves the right at its sole discretion to reject any and all bids, wholly or in part, to waive any informalities or any irregularities therein, to accept any bid even though it may not be the lowest bid, to call for rebids, to negotiate with any bidder, and to make an award which in its sole and absolute judgment will best serve the Town's interest. The Selectboard reserves the right to investigate the financial responsibility of any bidder to determine his or her ability to assure service throughout the term of the contract.

#8. PERFORMANCE

The Selectboard reserves the right to terminate any contract for poor performance, inadequate quality of product or service, or failure to meet timely delivery dates.

#9. CHANGE ORDERS

If specification changes are made prior to the close of the bid process, the Request for Bids will be amended and notice shall be sent to any bidder who already submitted a bid and a new bid process will be initiated. Once a bid has been accepted, if changes to the specifications become necessary, the Selectboard will prepare a change order specifying the scope of the change. Once approved, the contractor and an authorized agent of the Town must sign the change order.

#10. EXCEPTIONS

- Sole Source Purchases. If the Selectboard determines that there is only one possible source for a proposed purchase, it may waive the bid process and authorize the purchase from the sole source.
- Recurring Purchases. If the total value of a recurring purchase of a good or service is anticipated to exceed \$25,000.00 during any fiscal year, the bid process shall be utilized and shall specify the recurring nature of the purchase. Once a bid has been accepted, all future purchases shall be made from that bidder without necessity of additional bids, until such time as the Selectboard votes to initiate a new bid process.

#11. EMERGENCY PURCHASES

The Selectboard may award contracts and make purchases for the purpose of meeting the public emergency without complying with the bid process. Emergency expenditures may include immediate repair or maintenance of town property, vehicles, or equipment if the delay in such repair or maintenance would endanger persons or property or result in substantial impairment of the delivery of important Town services.

#12. PROFESSIONAL SERVICES.

The bid process shall not apply to the selection of providers for services that are characterized by a high degree of professional judgment and discretion including legal, financial, auditing, engineering, risk management, and insurance services.

The foregoing Policy is hereby adopted by the Selectboard of the Town of Halifax, Vermont, this 21st day of October, 2014 with amendments approved on the 7th day of October 2014 and is effective as of this date until amended or repealed.

Amended and approved by the Selectboard of the Town of Halifax this 16th day of August, 2016.



 Lewis Sumner, Chair



 Mitchell Green, Vice Chair



 Bradley Rafus